

PIGEON FORGE GREENWAY/BIKEWAY MASTER PLAN



TAPA Spring Retreat
March 22, 2019

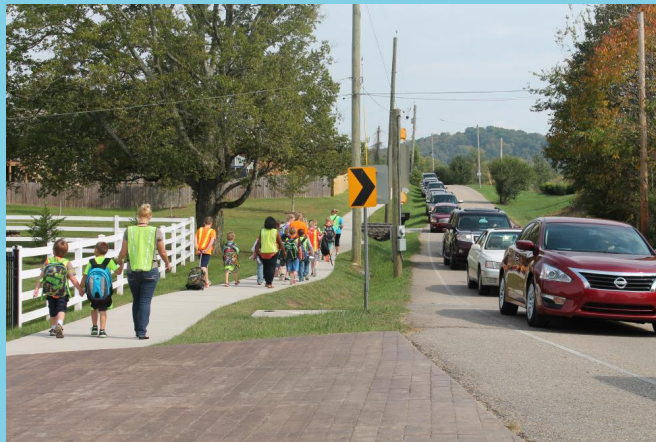
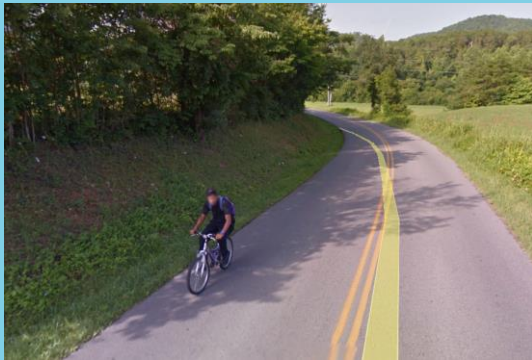
PIGEON FORGE GREENWAY/BIKEWAY **MASTER PLAN**

Lanny Goodwin, CPRP, Director of Parks and
Recreation
City of Pigeon Forge

Drew Gaskins, AICP
Gresham Smith, Nashville

Bert Kuyrkendall, PE, AICP
Gresham Smith, Chattanooga

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Greenways



Side Paths



Bicycle Lanes



Shared Lanes



Paved Shoulders



Intersections

WALKING & BICYCLING MEAN BUSINESS

"Walkable neighborhoods are driving **economic growth** as employers and businesses recognize the value of locating in these places to attract employees and customers."

- National Association of Realtors (2017)



THREE WAYS WALKING AND BICYCLING BENEFIT LOCAL ECONOMIES

1

INCREASE PROPERTY VALUES

Within five years of opening, properties within a block of the Indianapolis Cultural Trail saw their values increase by 148 percent. Nationally, home values in walkable neighborhoods are 15 percent higher than in non-walkable areas.

2

SUPPORT LOCAL BUSINESSES

In Dunedin, Florida, retail vacancies dropped 50 percent after the opening of the Pinellas Trail. Boulder, Colorado sees \$52 million in economic activity and 330 jobs from the city's bicycle industry.

3

ATTRACT TOURISTS

North Carolina's Outer Banks generates more than \$60 million annually in economic bicycle tourism, supporting 1,400 local jobs. The Silver Comet Trail in Georgia generates approximately \$120 million in total annual expenditures along its 66-mile length.



A Glorified Sidewalk, and the Path to Transform Atlanta

By RICHARD FAUSSET SEPT. 11, 2016



SLIDE SHOW | 8 Photos

Atlanta's BeltLine: An Ambitious Plan to Bring a City Together

Dustin Chambers for The New York Times

ATLANTA — Could this traffic-clogged Southern city, long derided as the epitome of suburban sprawl, really be discovering its walkable, bike-friendly, density-embracing, streetcar-riding, human-scale soul?

The answer is evident in the outpouring of affection that residents here have showered on the Atlanta BeltLine, which aims to convert 22 miles of mostly disused railway beds circling the city's urban core into a biking and pedestrian loop, a new streetcar line, and a staggeringly ambitious engine of urban revitalization.

Even though just a small fraction of the loop trail has been completed, Atlanta is one of the super expressions of America's newly walkable

"Private investment along the entire proposed route has surged to \$3 billion."

BICYCLING IN
NORTHWEST ARKANSAS
PROVIDED



\$137 MILLION

in benefits to the economy in 2017



\$86 MILLION

in health care-related costs



\$51 MILLION

in business benefits

Source: Economic and Health Benefits of Bicycling in Northwest Arkansas



CYCLING PROVIDED ABOUT
\$27 MILLION
IN NORTHWEST ARKANSAS
TOURISM SPENDING BY
OUT-OF-STATE VISITORS IN 2017

Source: Economic and Health Benefits of Bicycling in Northwest Arkansas



NEARLY **1/3** OF NORTHWEST
**ARKANSAS
RESIDENTS**
CONSIDER THE AVAILABILITY OF PAVED BIKE INFRASTRUCTURE
IMPORTANT IN DECIDING **WHERE TO LIVE**



Source: Economic and Health Benefits of Bicycling in Northwest Arkansas



**ANNUAL BIKE USAGE
IN NORTHWEST ARKANSAS
INCREASED BY 24%**
BETWEEN 2015 AND 2017



2017 Northwest Arkansas Trail Usage Monitoring Report



Provide Access to Parks and Natural Areas



Improve Connections Among Key Activity Centers



Enhance Bicycle and Pedestrian Access Along the Parkway and Across it at Key Locations



Improve Connections Among Schools and Public Facilities



Link Local Campgrounds and RV Parks to Greenways and Bikeways

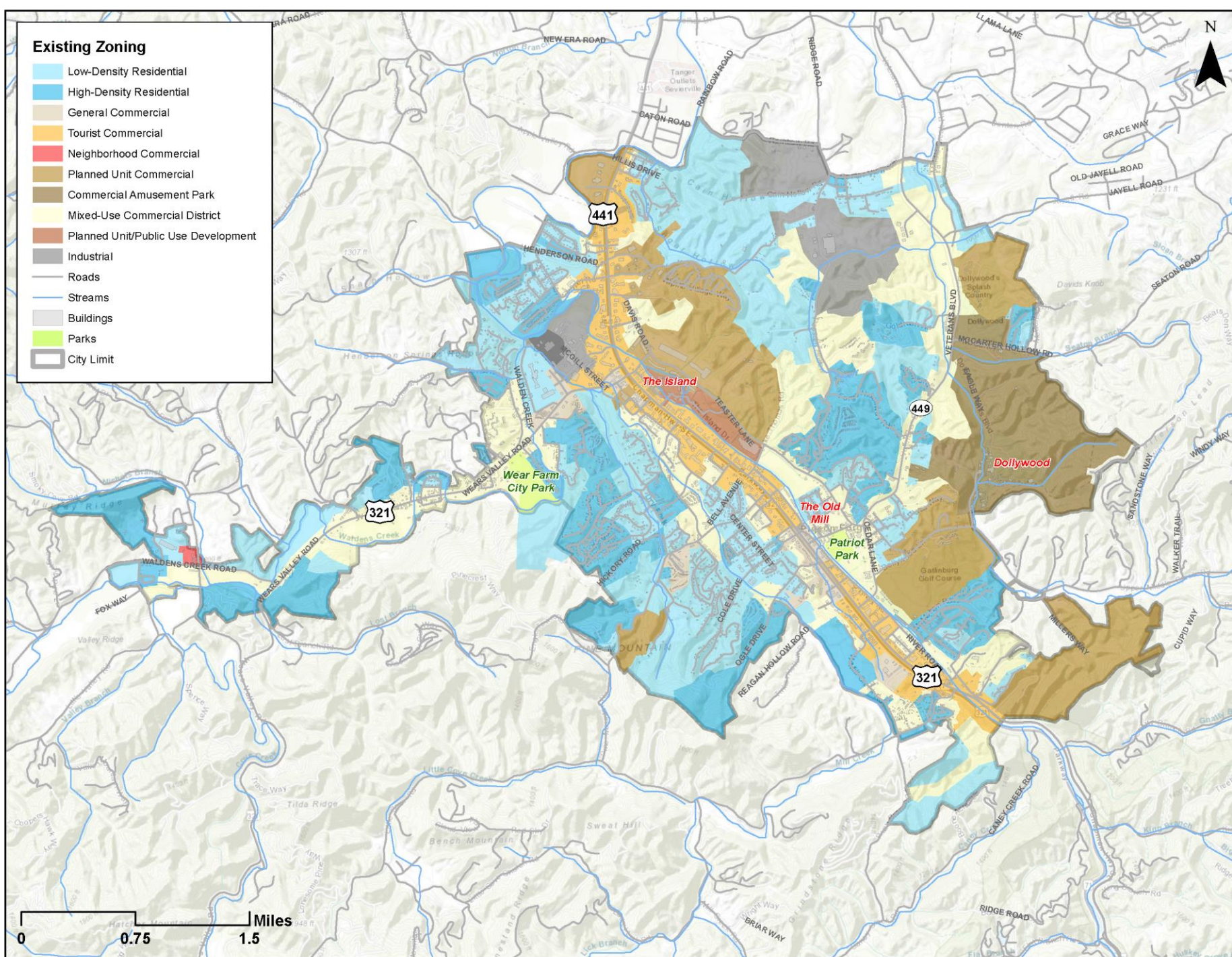


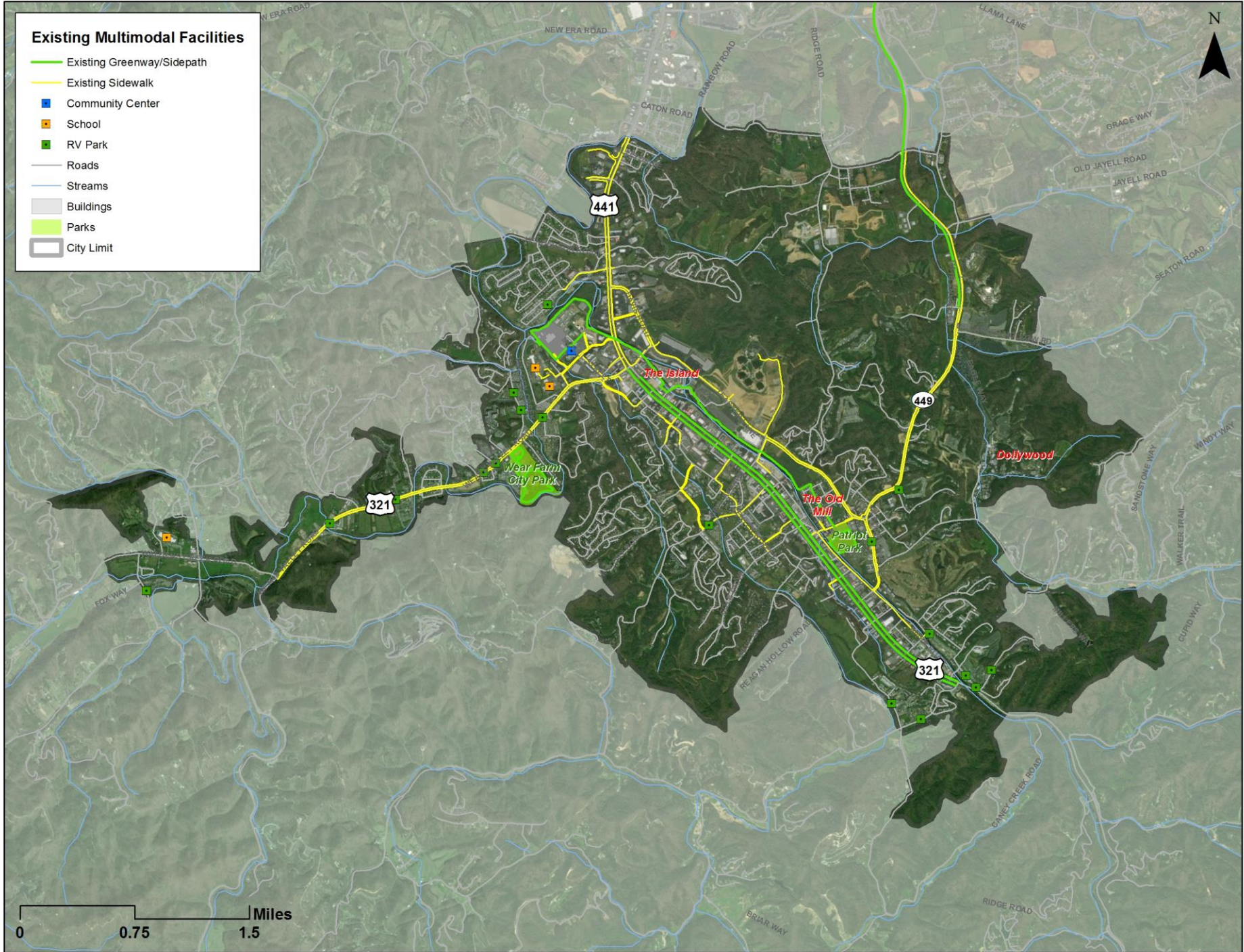
Incorporate Local Greenways and Bikeways into a Larger Regional Network



Existing Zoning

-  Low-Density Residential
-  High-Density Residential
-  General Commercial
-  Tourist Commercial
-  Neighborhood Commercial
-  Planned Unit Commercial
-  Commercial Amusement Park
-  Mixed-Use Commercial District
-  Planned Unit/Public Use Development
-  Industrial
-  Roads
-  Streams
-  Buildings
-  Parks
-  City Limit





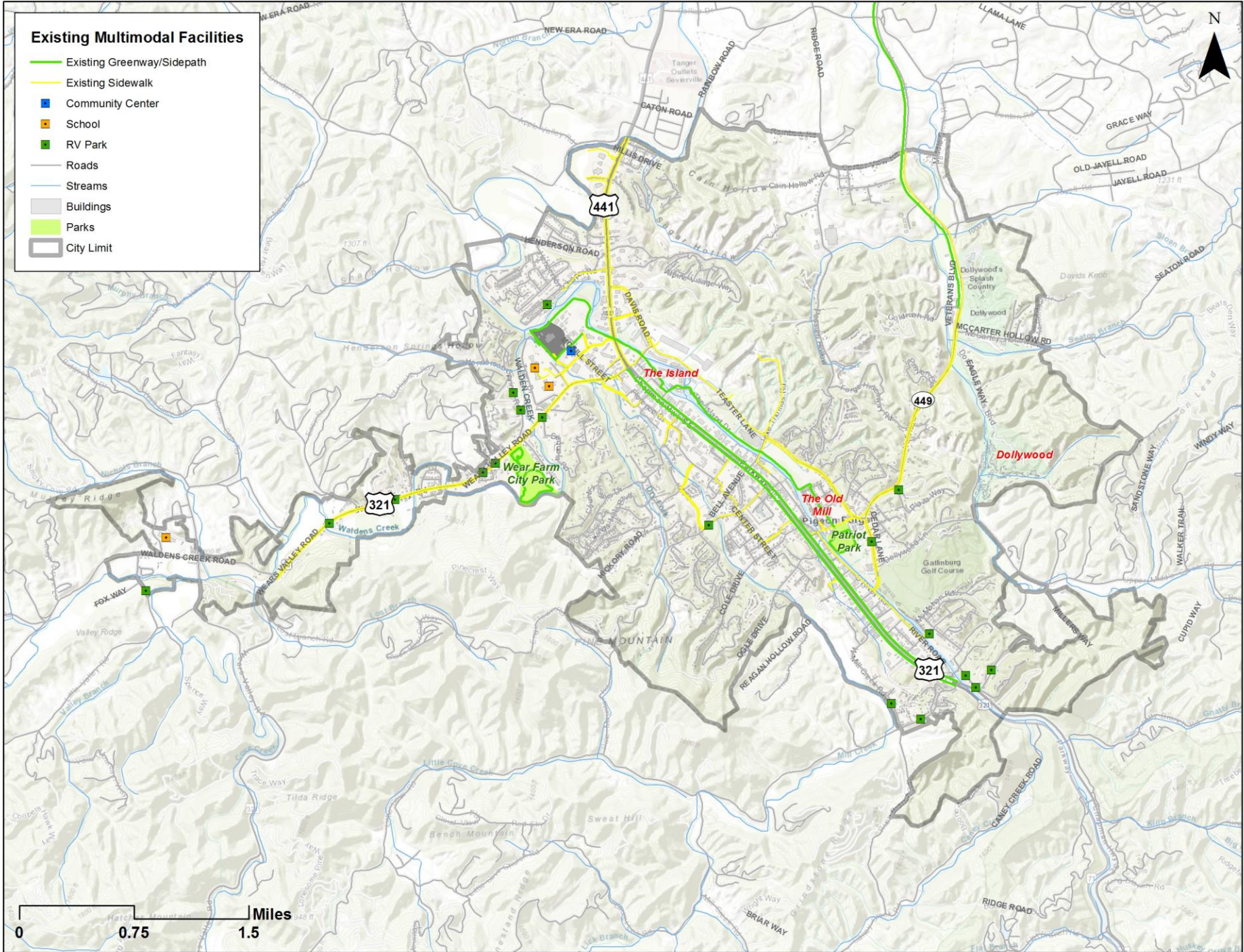
Existing Multimodal Facilities

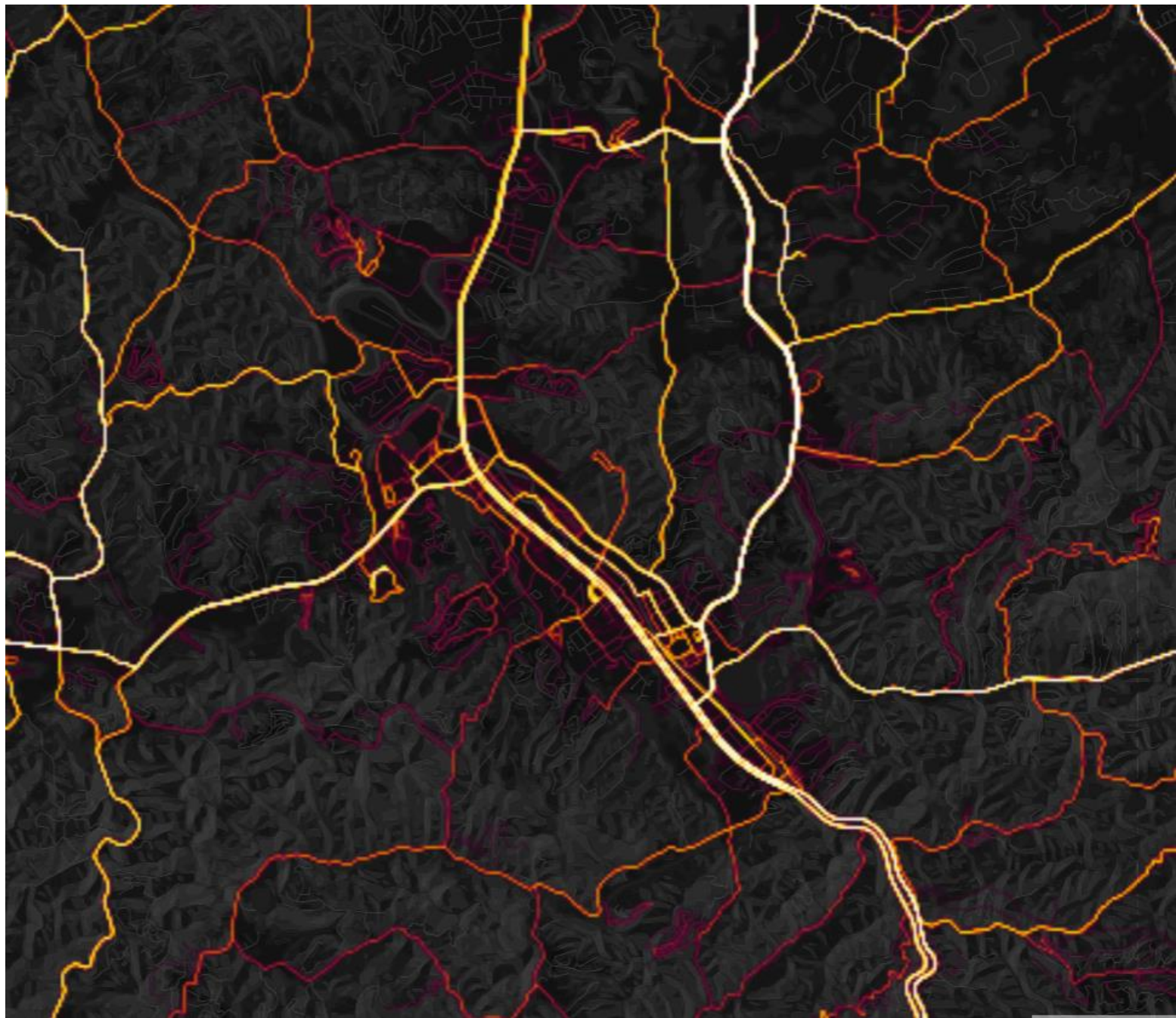
- Existing Greenway/Sidepath
- Existing Sidewalk
- Community Center
- School
- RV Park
- Roads
- Streams
- Buildings
- Parks
- City Limit

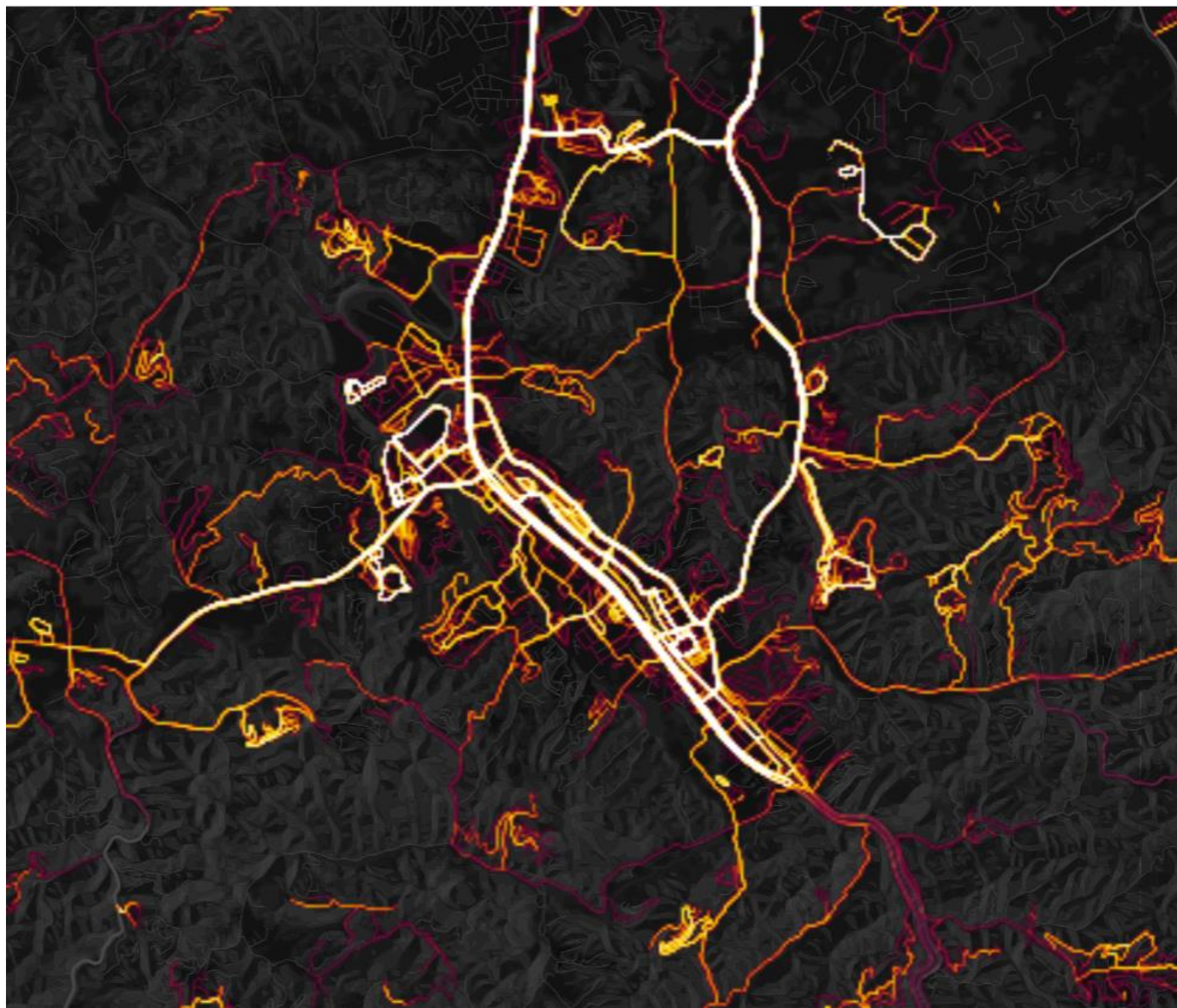
0 0.75 1.5 Miles

Map of Wear Farm City, Georgia, showing existing multimodal facilities. The map includes a legend, a scale bar, and a north arrow. Key features include existing greenways/sidepaths (green lines), existing sidewalks (yellow lines), community centers (blue squares), schools (orange squares), RV parks (green squares), roads (grey lines), streams (blue lines), buildings (grey shapes), parks (green areas), and the city limit (black outline). Specific locations marked include 'The Island', 'The Old Mill', 'Patriot Park', 'Wear Farm City Park', 'Dollywood', and 'Waldens Creek'. Major roads shown are US-441, US-321, and GA-449.













-
- Existing Multimodal Facilities**
- Existing Greenway/Sidepath
 - Existing Sidewalk
 - Community Center
 - School
 - RV Park
 - Roads
 - Streams
 - Buildings
 - Parks
 - City Limit
- 0 0.75 1.5 Miles
- Wear Farm City, North Carolina
- Key locations and roads shown on the map include:
- Roads:** US-441, US-321, NC-449, Henderson Road, Davis Road, Tester Lane, Bell Avenue, Center Street, Cole Drive, Regan Hollow Road, Ridge Road, Seaton Road, Old Javell Road, Javell Road, Grace Way, Veterans Blvd, Dollywood Hwy, Sandstone Way, Walker Trail, Curd Way, Briar Way, Ridge Road, Caney Creek Road, Mill Creek, Little Cove Creek, Waldens Creek, Fox Way, Waldens Creek Road, New Era Road, Baton Road, Rainbow Road, Hillis Drive, Millis Drive, Henderson Road, Davis Road, Tester Lane, Bell Avenue, Center Street, Cole Drive, Regan Hollow Road, Ridge Road, Seaton Road, Old Javell Road, Javell Road, Grace Way, Veterans Blvd, Dollywood Hwy, Sandstone Way, Walker Trail, Curd Way, Briar Way, Ridge Road, Caney Creek Road, Mill Creek, Little Cove Creek, Waldens Creek, Fox Way, Waldens Creek Road.
 - Streams:** Waldens Creek, Little Cove Creek, Mill Creek, Little Cove Creek, Waldens Creek, Fox Way, Waldens Creek Road.
 - Parks:** Wear Farm City Park, Patriot Park, Dollywood, The Island, The Old Mill.
 - Other Features:** Community Center, School, RV Park, Buildings, City Limit.

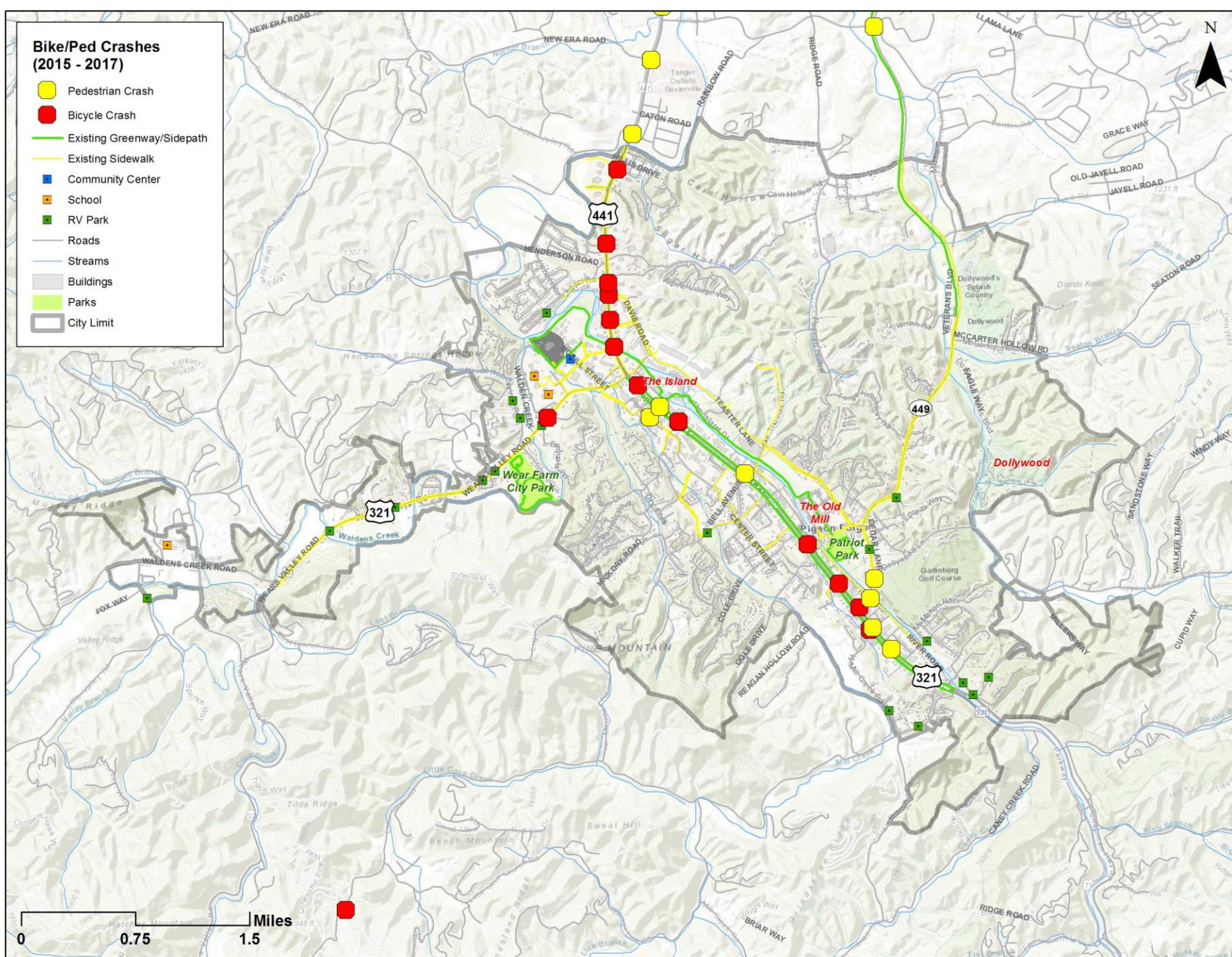






Bike/Ped Crashes (2015 - 2017)












-  Pedestrian Crash
-  Bicycle Crash
-  Existing Greenway/Sidepath
-  Existing Sidewalk
-  Community Center
-  School
-  RV Park
-  Roads
-  Streams
-  Buildings
-  Parks
-  City Limit

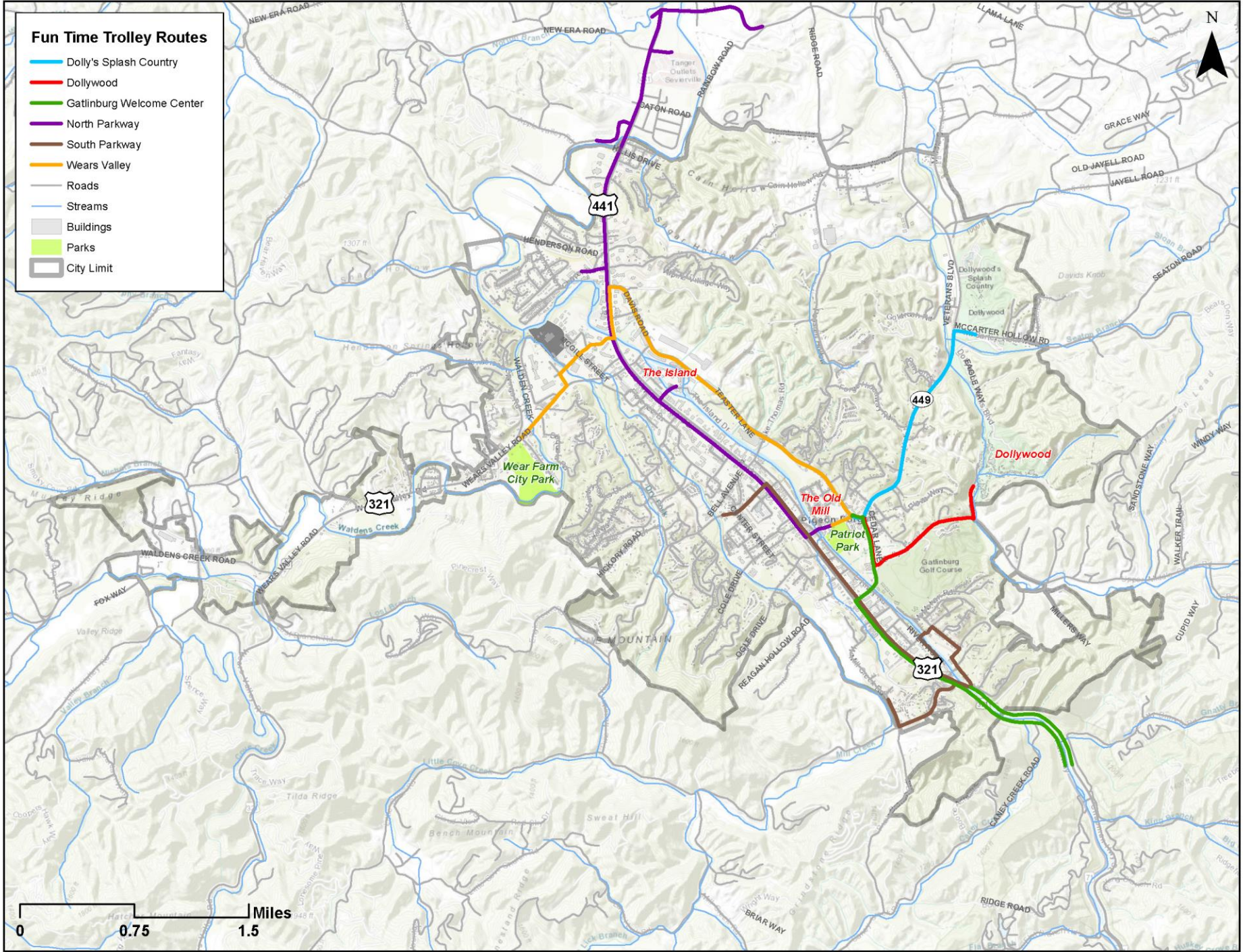


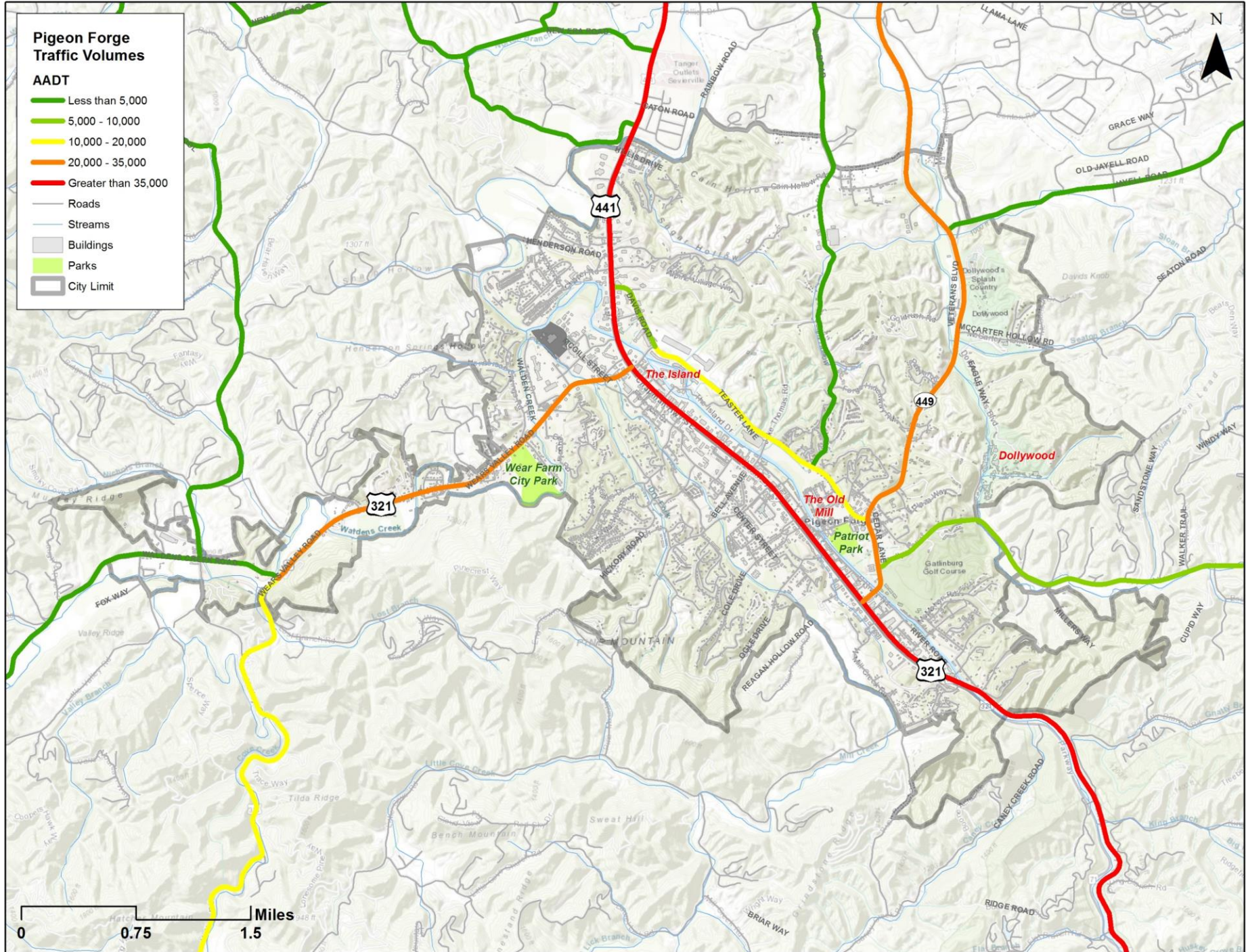
Fun Time Trolley Routes

- Dolly's Splash Country
- Dollywood
- Gatlinburg Welcome Center
- North Parkway
- South Parkway
- Wears Valley
- Roads
- Streams
- Buildings
- Parks
- City Limit

Map showing Fun Time Trolley Routes in Sevier County, Tennessee. The routes are color-coded: Dolly's Splash Country (blue), Dollywood (red), Gatlinburg Welcome Center (green), North Parkway (purple), and South Parkway (brown). The map also shows Wears Valley (orange), roads, streams, buildings, parks, and the city limit. Key locations include Dollywood, The Island, The Old Mill, Patriot Park, and Wear Farm City Park. A scale bar indicates 0 to 1.5 miles, and a north arrow is present.

-  Dolly's Splash Country
 Dollywood
 Gatlinburg Welcome Center
 North Parkway
 South Parkway
 Wears Valley
 Roads
 Streams
 Buildings
 Parks
 City Limit





Greenways and Bikeways Master Plan

Existing Facilities

- Existing Greenway/Sidepath
- Existing Sidewalk

Proposed Facilities

- Proposed Bike Lane
- Proposed Shared Lane/Bike Boulevard
- Proposed Greenway/Sidepath
- Proposed Unpaved Trail

- Intersection Safety Improvements

- Community Center

- School

- RV Park

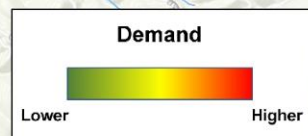
- Roads

- Streams

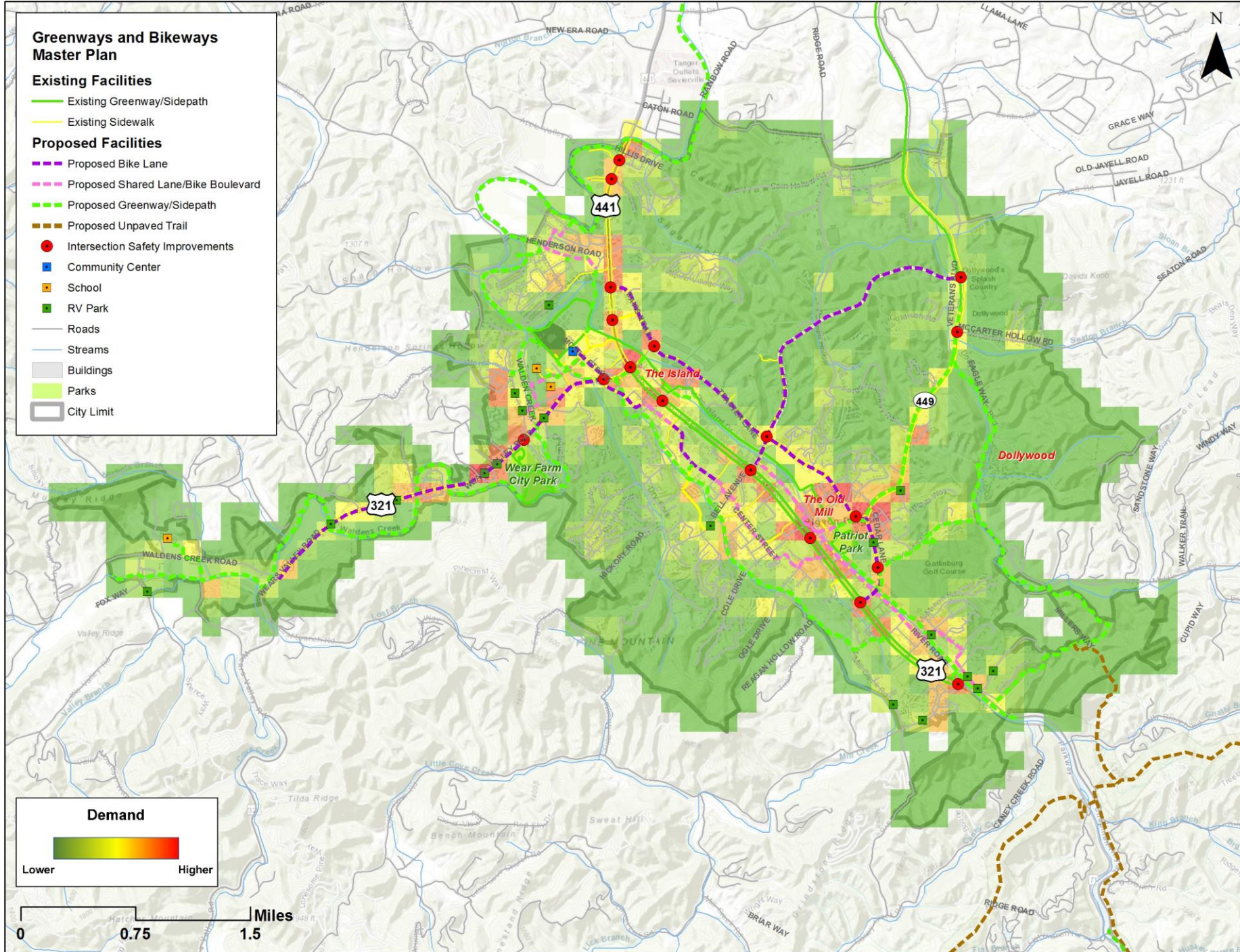
- Buildings

- Parks

- City Limit



0 0.75 1.5 Miles



Greenways and Bikeways Master Plan

Existing Facilities

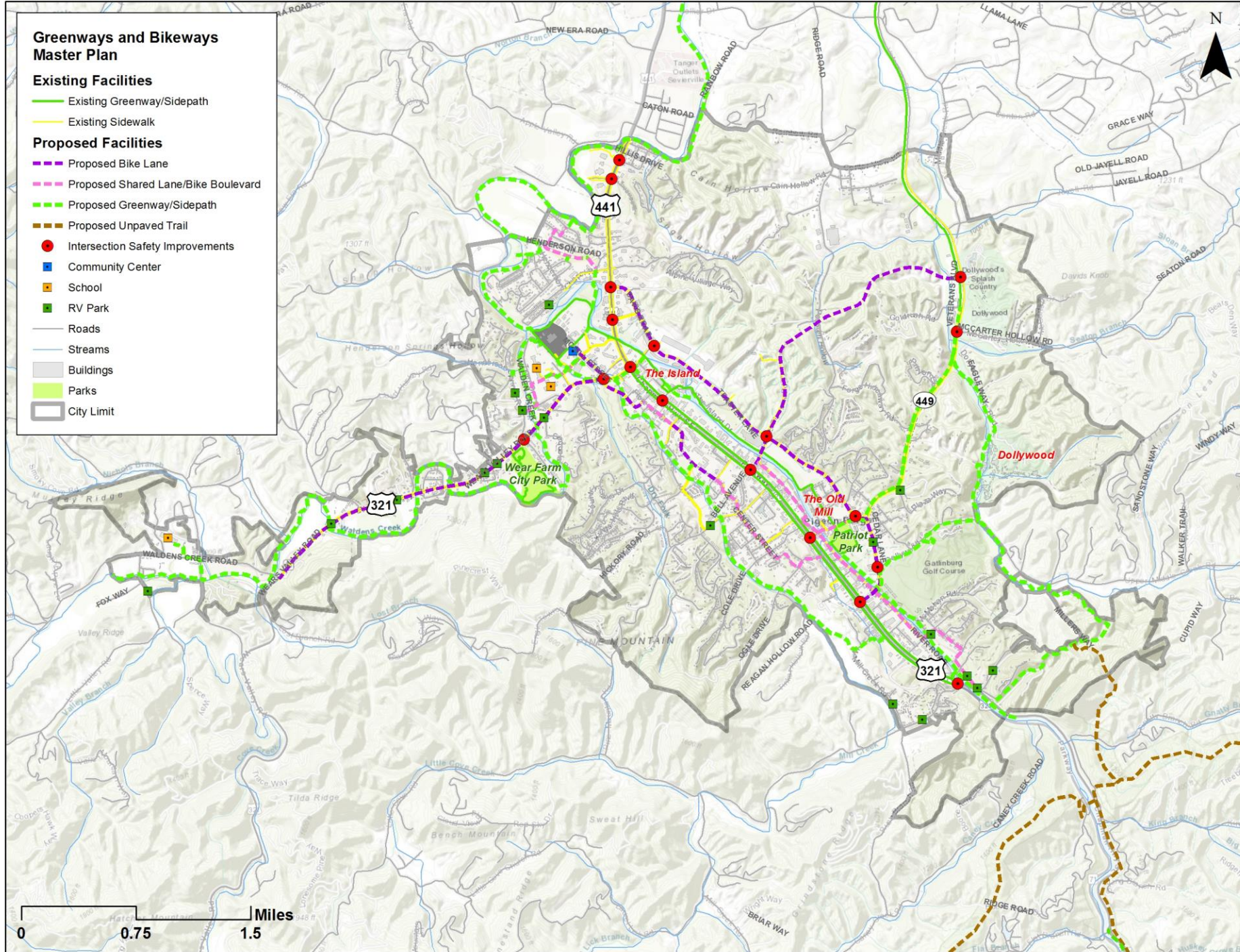
- Existing Greenway/Sidepath
- Existing Sidewalk

Proposed Facilities

- Proposed Bike Lane
- Proposed Shared Lane/Bike Boulevard
- Proposed Greenway/Sidepath
- Proposed Unpaved Trail
- Intersection Safety Improvements

- Community Center
- School
- RV Park

- Roads
- Streams
- Buildings
- Parks
- City Limit



Greenways and Bikeways Master Plan

Existing Facilities

- Existing Greenway/Sidepath
- Existing Sidewalk

Proposed Facilities

- Proposed Bike Lane
- Proposed Shared Lane/Bike Boulevard
- Proposed Greenway/Sidepath
- Proposed Unpaved Trail

- Intersection Safety Improvements

- Community Center

- School

- RV Park

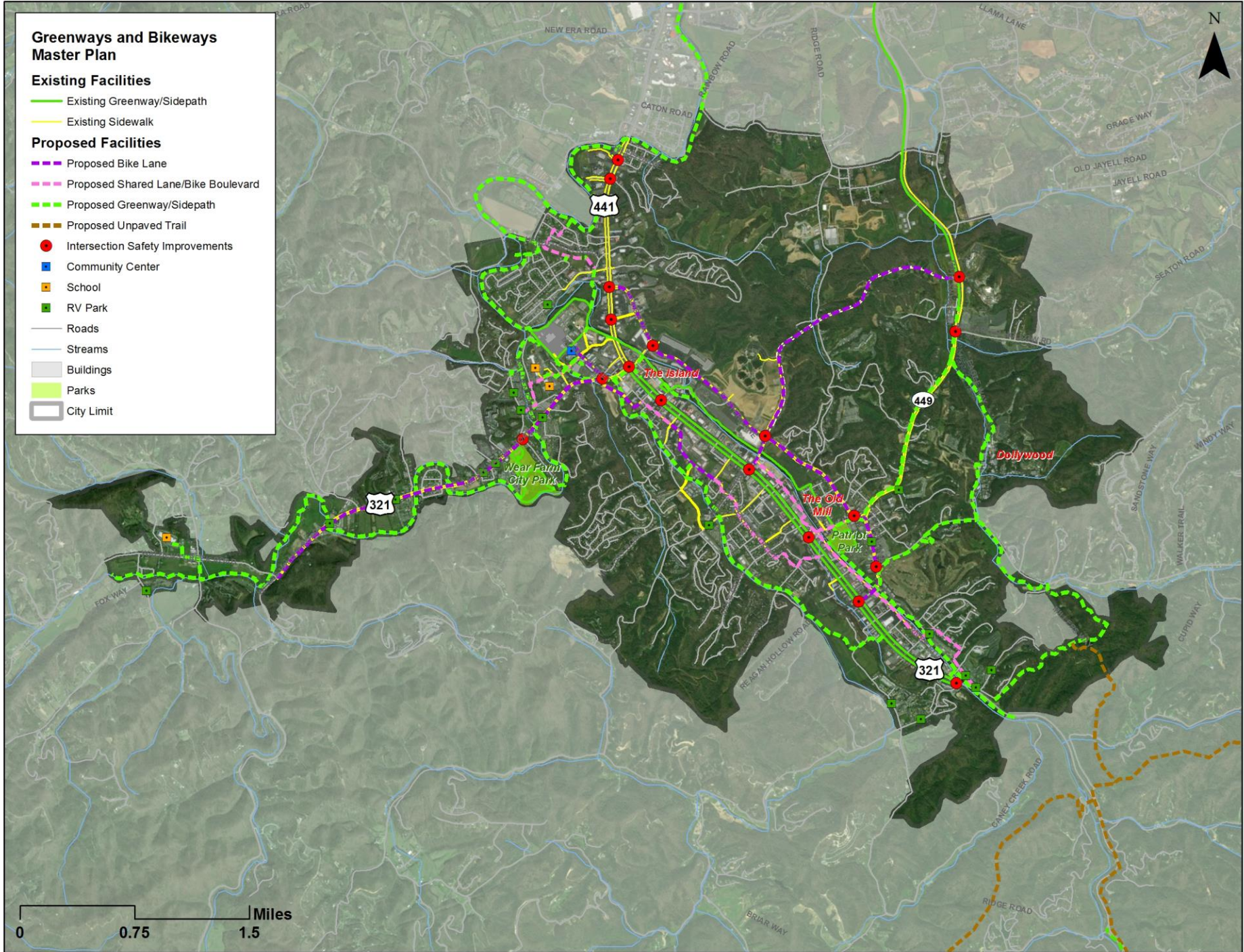
- Roads

- Streams

- Buildings

- Parks

- City Limit



Waldens Creek Greenway - Phase 1 Project Location

Existing Facilities

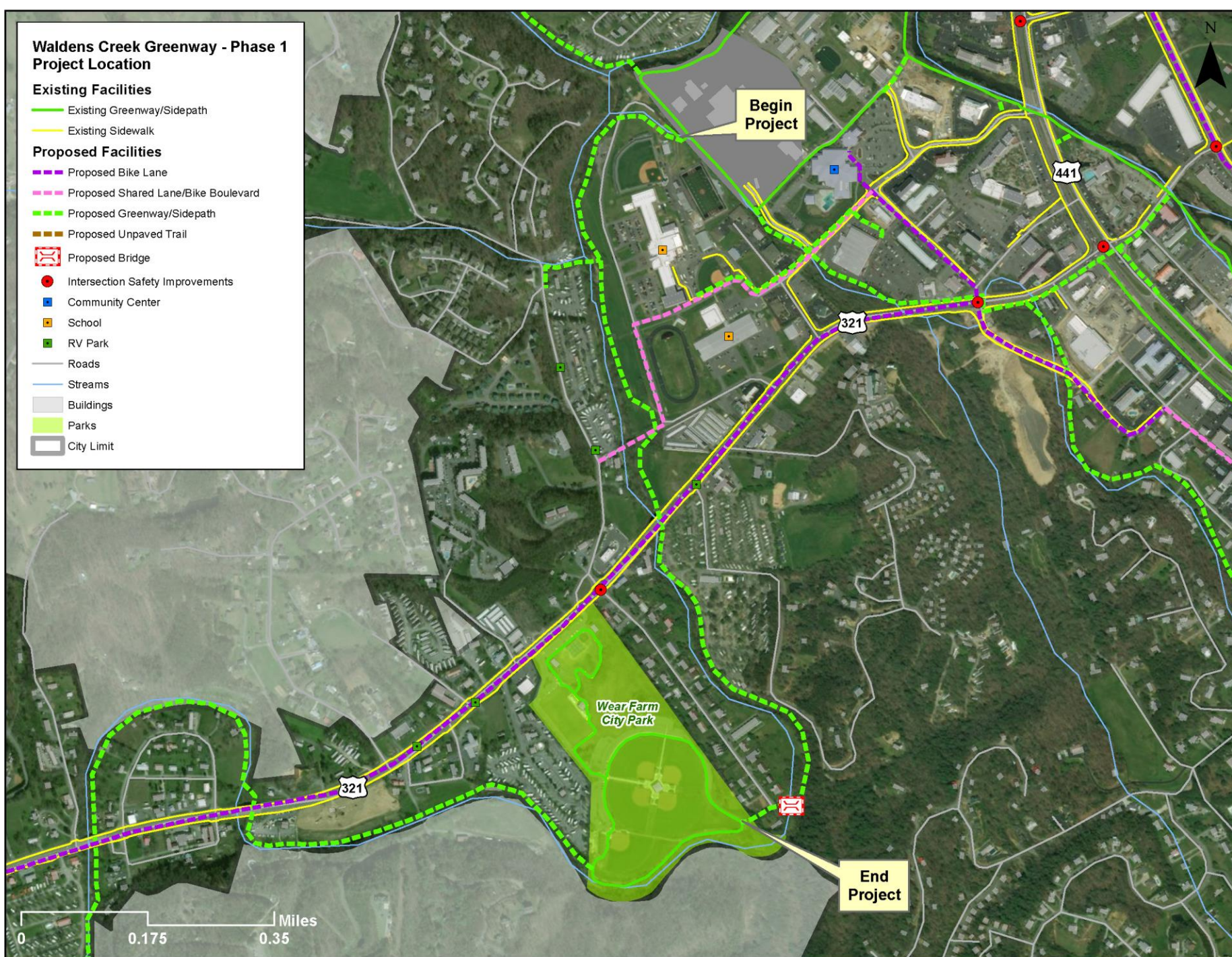
- Existing Greenway/Sidepath
- Existing Sidewalk

Proposed Facilities

- Proposed Bike Lane
- Proposed Shared Lane/Bike Boulevard
- Proposed Greenway/Sidepath
- Proposed Unpaved Trail
- Proposed Bridge

- Intersection Safety Improvements
- Community Center
- School
- RV Park

- Roads
- Streams
- Buildings
- Parks
- City Limit





PROPOSED GREENWAY

STORMWATER BUFFER (TYP)

WALDENS CREEK

PARKWAY AND WEARS VALLEY



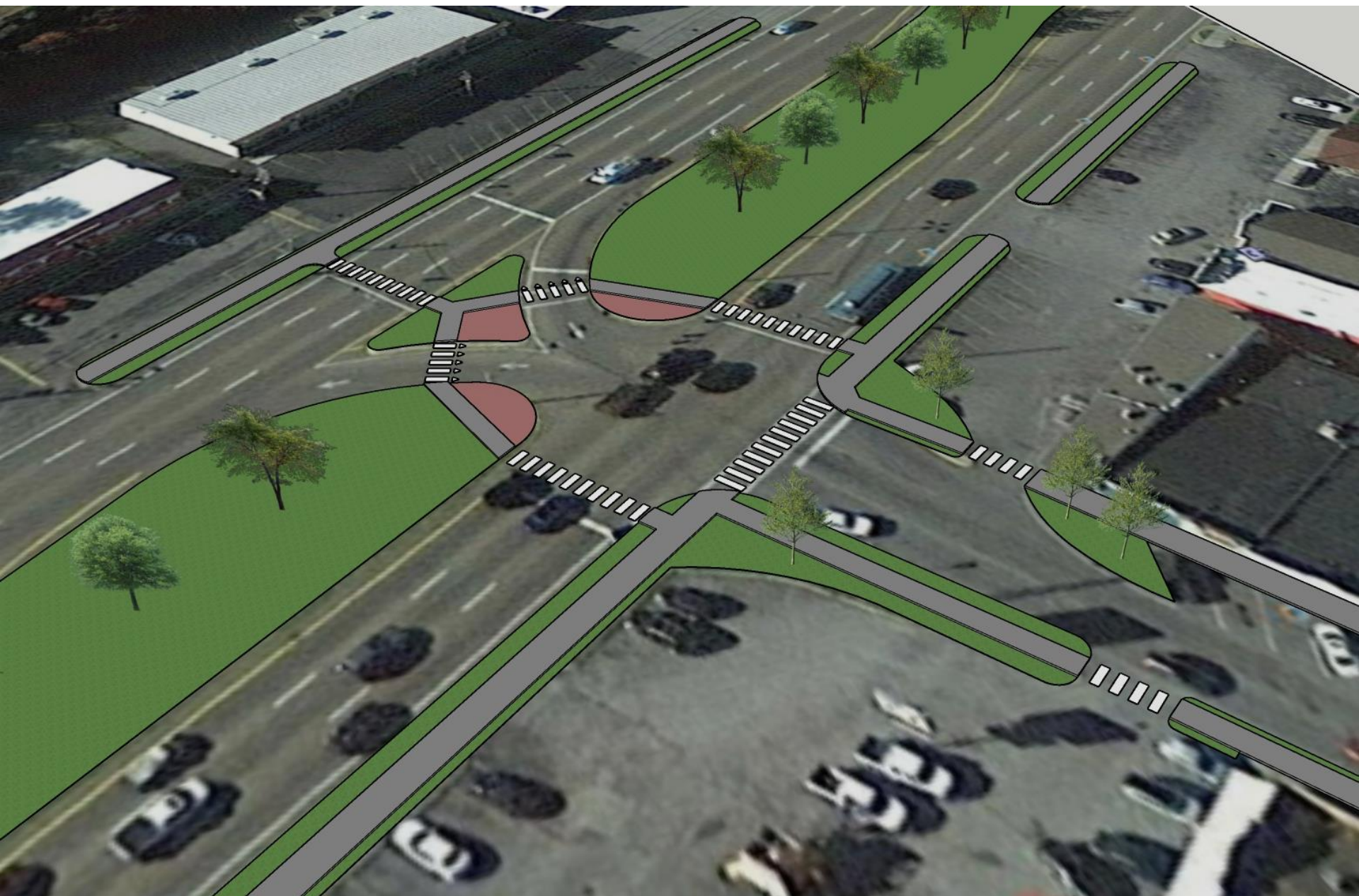
PARKWAY AND WEARS VALLEY



PARKWAY AND OLD MILL



PARKWAY AND OLD MILL

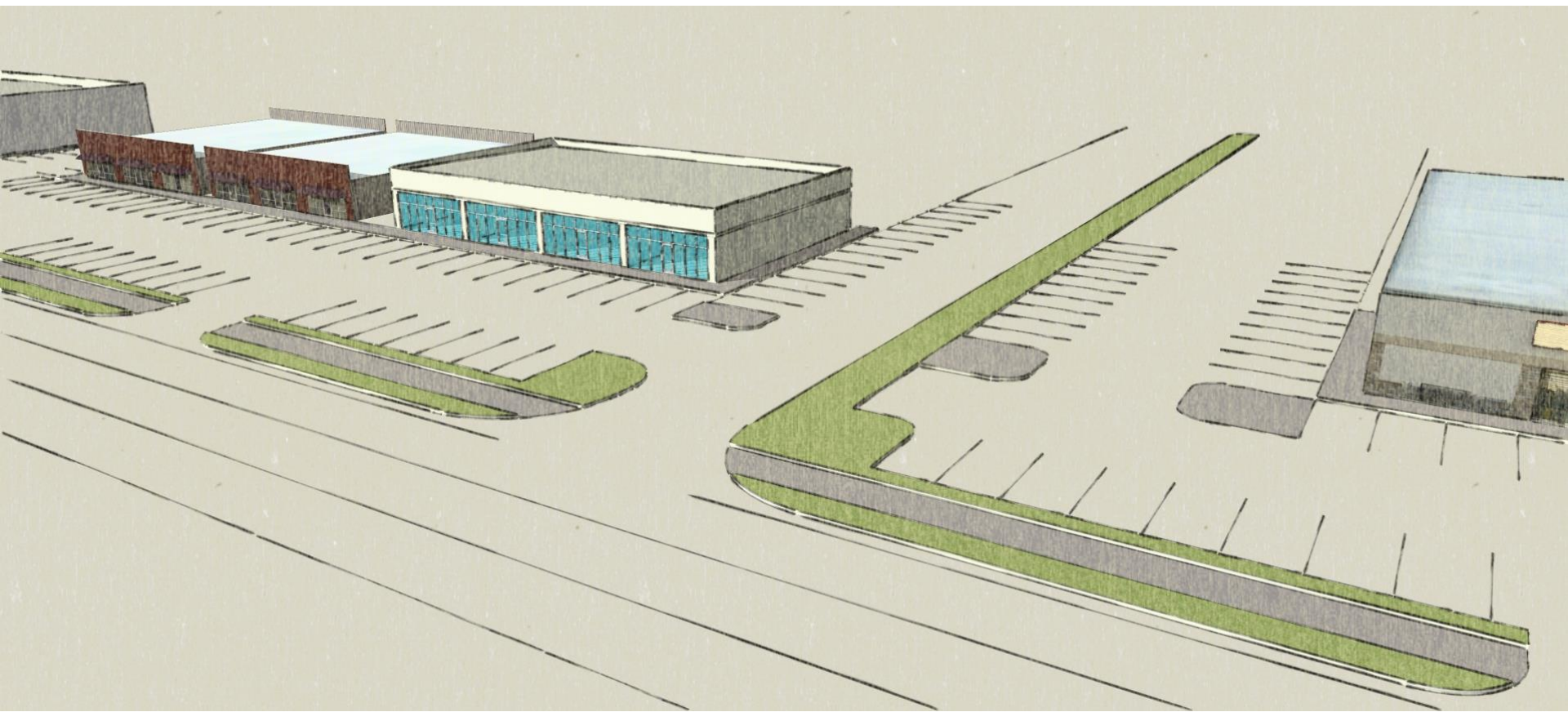


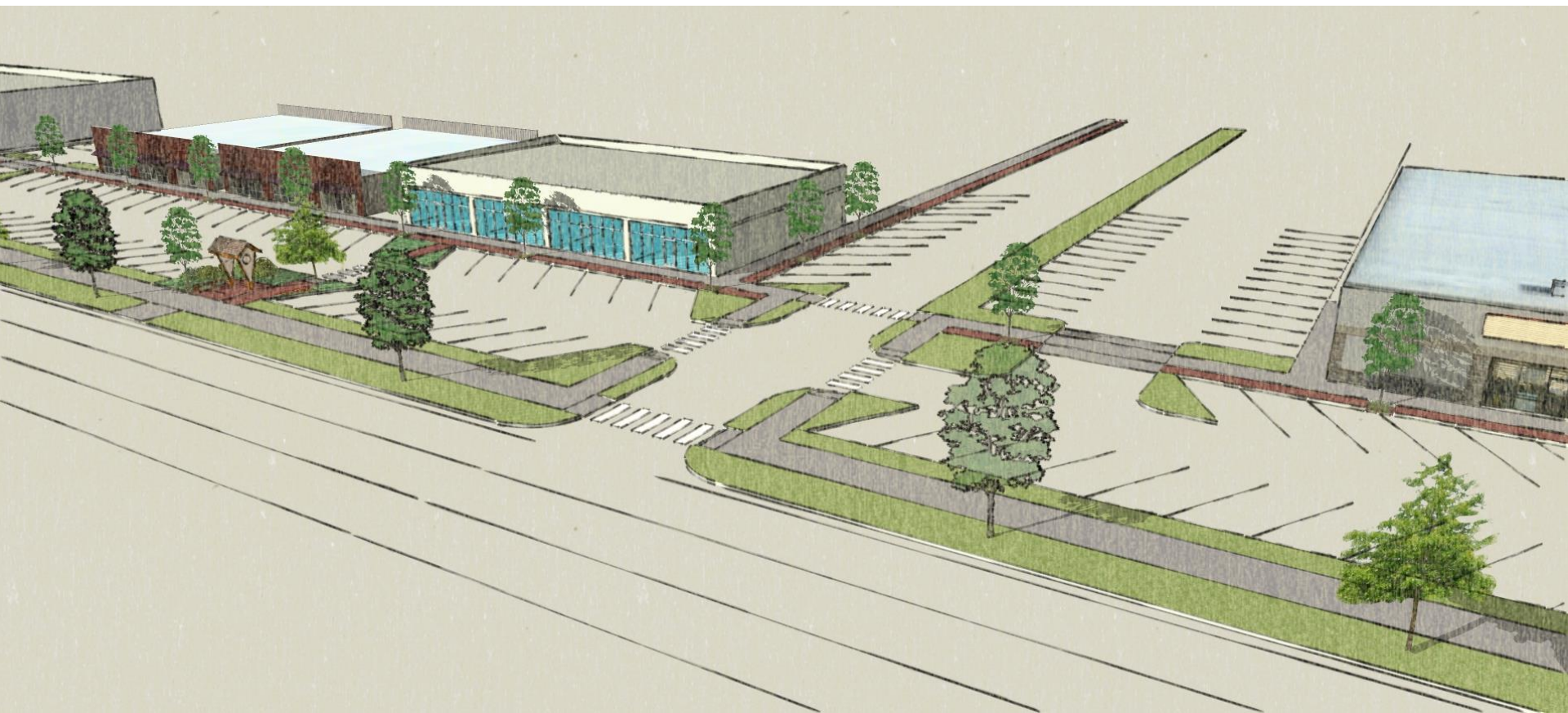
VETERANS AND MCCARTER HOLLOW



VETERANS AND MCCARTER HOLLOW











Google Earth



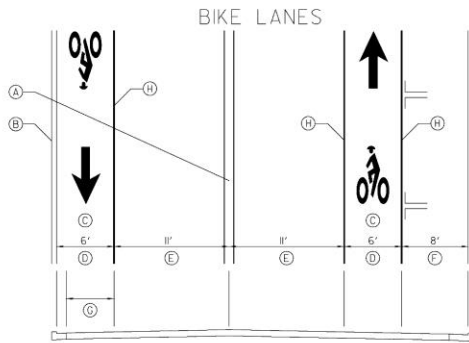
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Google Earth



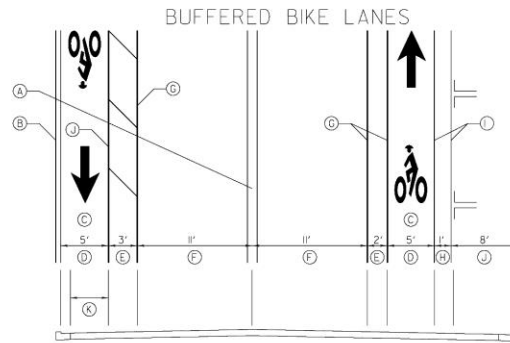






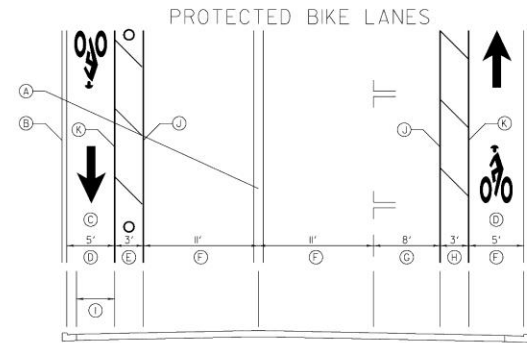
LEGEND

- | | | |
|--|-------------------------------------|---|
| (A) CENTERLINE** | (D) BICYCLE LANE - 6' MAX/5' MIN*** | (G) DISTANCE FROM GUTTER PAN TO EDGE STRIPE - 4' MAX/3' MIN |
| (B) CURB & GUTTER OR HEADER CURB | (E) TRAVEL LANE - 12' MAX/10' MIN | (H) 6"-TO-8" WHITE STRIPE |
| (C) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3) | (F) PARKING LANE - 8' MAX/7' MIN | |



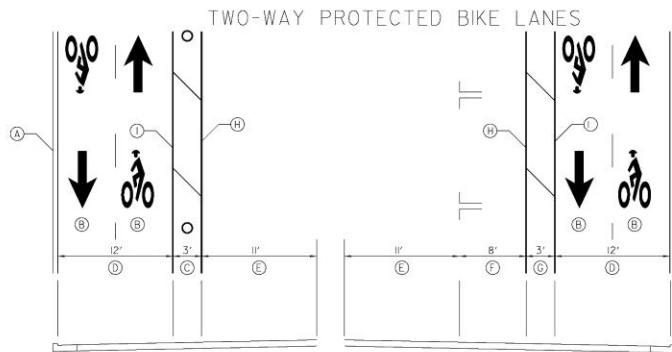
LEGEND

- | | | | |
|--|------------------------------------|---------------------------|---|
| (A) CENTERLINE** | (D) BICYCLE LANE - 5' MAX/5' MIN | (H) 6"-TO-8" WHITE STRIPE | (J) PARKING LANE - 8' MAX/7' MIN |
| (B) CURB & GUTTER OR HEADER CURB | (E) BUFFER ZONE - 3' MAX/1.5' MIN* | (I) 4" WHITE STRIPE | (K) DISTANCE FROM GUTTER PAN TO EDGE STRIPE - 4' MAX/3' MIN |
| (C) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3) | (F) TRAVEL LANE - 12' MAX/10' MIN | | |



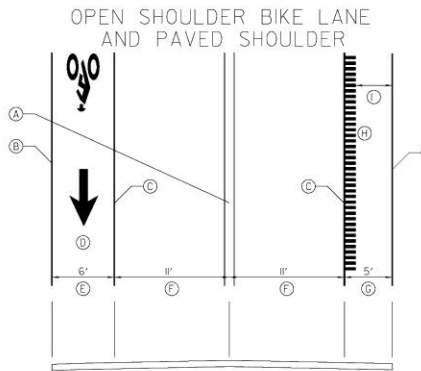
LEGEND

- | | | |
|--|---|---|
| (A) CENTERLINE** | (E) BUFFER ZONE WITH FLEXIBLE BOLLARDS* | (I) DISTANCE FROM GUTTER PAN TO EDGE STRIPE - 4' MAX/3' MIN |
| (B) CURB & GUTTER OR HEADER CURB | (F) TRAVEL LANE - 12' MAX/10' MIN | (J) 6"-TO-8" WHITE STRIPE |
| (C) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3) | (G) PARKING LANE - 8' MAX/7' MIN | (K) 4" WHITE STRIPE |
| (D) BICYCLE LANE - 5' MAX/5' MIN | (H) BUFFER ZONE - 3' MIN* | |



LEGEND

- | | | |
|--|------------------------------------|---|
| (A) CURB & GUTTER OR HEADER CURB | (D) BICYCLE LANES - 12' MAX/8' MIN | (G) BUFFER ZONE WITH DIAGONAL CROSS-HATCHING - 3' MIN |
| (B) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3) | (E) TRAVEL LANE - 12' MAX/10' MIN | (H) 6"-TO-8" WHITE STRIPE |
| (C) BUFFER ZONE WITH FLEXIBLE BOLLARDS* | (F) PARKING LANE - 8' MAX/7' MIN | (I) 4" WHITE STRIPE |



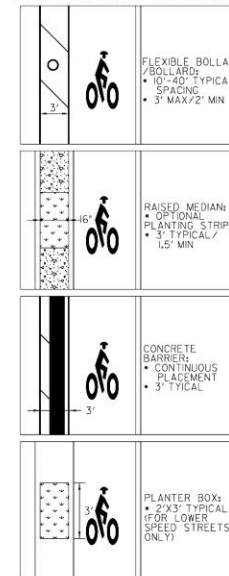
LEGEND

- | | | |
|----------------------------|--|------------------------------------|
| (A) CENTERLINE** | (D) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3) | (G) PAVED SHOULDER - 8' MAX/5' MIN |
| (B) ADVISORY SHOULDER LINE | (E) BICYCLE LANE - 6' PREFERRED/ 5' MIN | (H) RUMBLE STRIPS (OPTIONAL)*** |
| (C) 6"-TO-8" WHITE STRIPE | (F) TRAVEL LANE - 12' MAX/10' MIN | (I) RIDEABLE SPACE - 4' MIN |

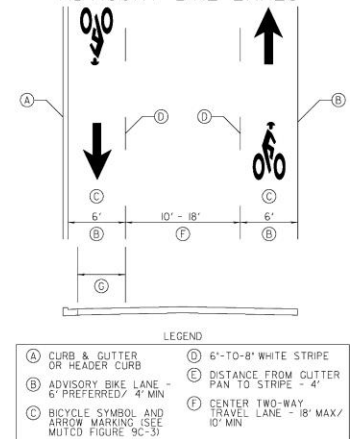
NOTES

- * DIAGONAL HATCHING REQUIRED IF BUFFER > 3', ANGLE 30° TO 45°, SPACING 10' TO 40'
- ** NOT REQUIRED IF ADT<3,000
- *** EVERY 50' PROVIDE 12' GAP
- ADJACENT TO NARROW PARKING LANES (7'), A WIDER BICYCLE LANE (6'-7') PROVIDES MORE OPERATING SPACE TO AVOID OPENING VEHICLE DOORS

PROTECTION METHODS



ADVISORY BIKE LANES



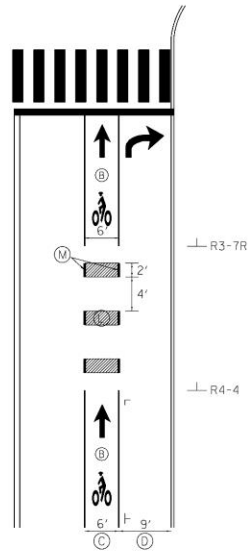
LEGEND

- | | | |
|--|---|---|
| (A) CURB & GUTTER OR HEADER CURB | (D) 6"-TO-8" WHITE STRIPE | (G) DISTANCE FROM GUTTER PAN TO STRIPE - 4' |
| (B) ADVISORY BIKE LANE - 6' PREFERRED/ 4' MIN | (E) CENTER TWO-WAY TRAVEL LANE - 18' MAX/ 10' MIN | |
| (C) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3) | | |

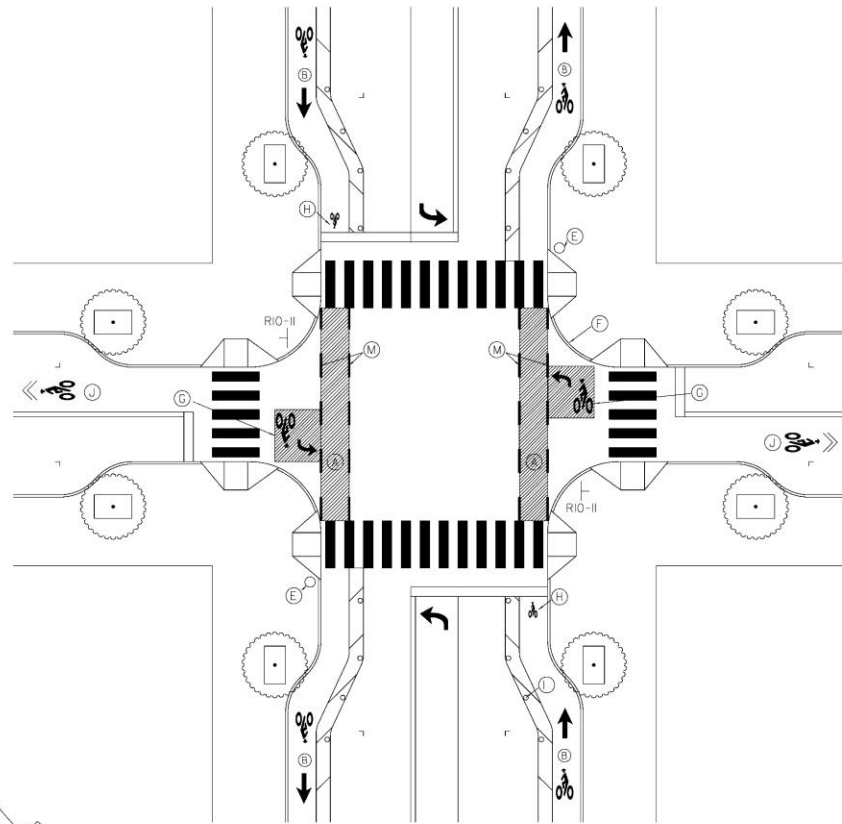
DRAFT

 PIGEON FORGE	BICYCLE FACILITIES DETAILS	PIGEON FORGE	BF-1
		FILE: STANDARD.dgn	SHEET NUMBER
		DATE: 3/2/2018	1

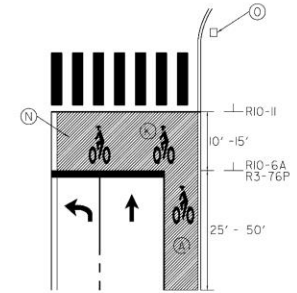
THROUGH LANE DETAIL



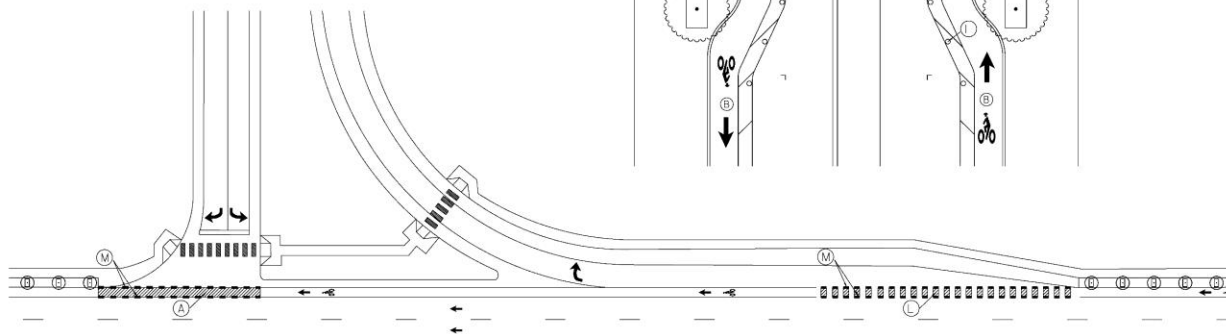
SIGNALIZED INTERSECTION DETAIL



BIKE BOX DETAIL
AT SIGNALIZED INTERSECTION



INTERCHANGE RAMP



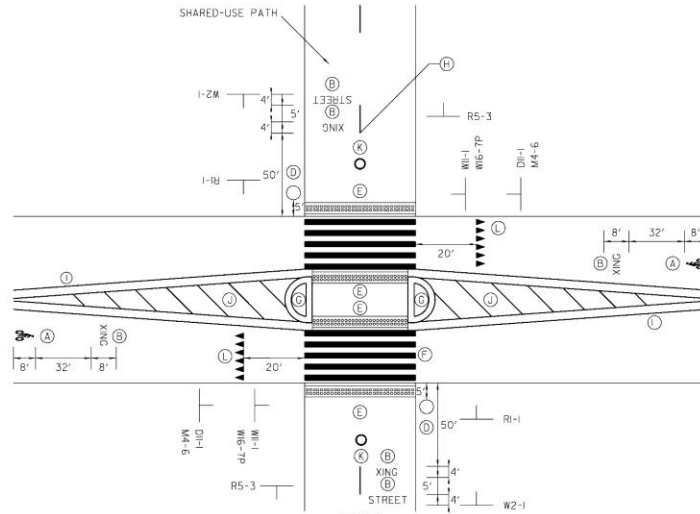
LEGEND

- (A) SOLID GREEN PAVEMENT MARKING (OPTIONAL)
- (B) BICYCLE SYMBOL AND ARROW MARKING (SEE MUTCD FIGURE 9C-3)
- (C) BICYCLE LANE - 6' MAX/5' MIN
- (D) PARKING LANE/TURNING LANE - 9' MIN
- (E) BICYCLE SIGNAL HEAD
- (F) 15' RADIUS TYP.
- (G) 2 STAGE TURN BOX WITH SOLID GREEN PAVEMENT MARKING (REFER TO MUTCD 1A-20)
- (H) BICYCLE DETECTION AND PAVEMENT MARKING (SEE MUTCD FIGURE 9C-7)
- (I) FLEXIBLE BOLLARD
- (J) SHARED LANE MARKING (SEE MUTCD FIGURE 9C-5)
- (K) BICYCLE SYMBOL (SEE MUTCD FIGURE 9C-3)
- (L) DASHED GREEN PAVEMENT MARKING
- (M) DASHED 6" OR 8" WHITE STRIPING
- (N) BICYCLE BOX (REFER TO MUTCD 1A-18)
- (O) PEDESTRIAN COUNTDOWN TIMER - TO BE LOCATED AT EACH CORNER OF THE INTERSECTION

DRAFT

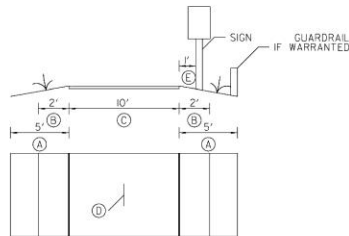
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		FILE: STANDARD.dgn	
		DATE: 3/2/2018	

SHARED-USE PATHWAY MID-BLOCK CROSSING



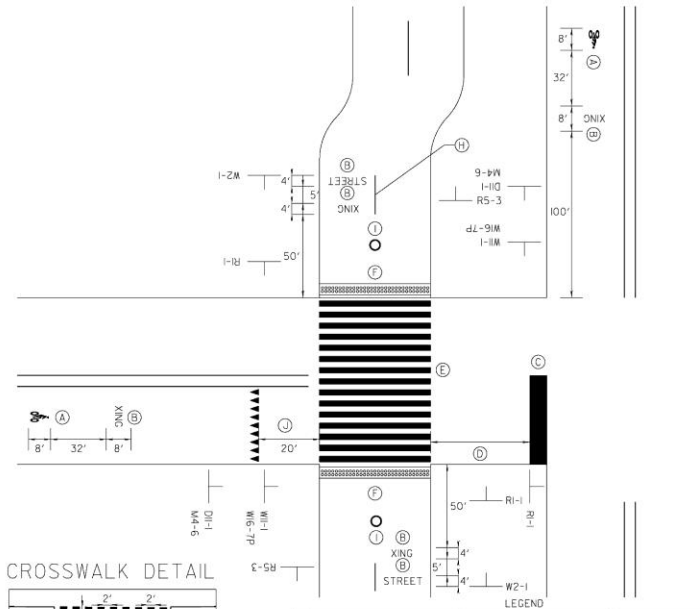
- | | | | |
|--|---|---|---|
| (A) BICYCLE SYMBOL
(SEE MUTCD -
FIG. 9C-3) | (D) PEDESTRIAN HYBRID
BEACON IF WARRANTED | (G) RAISED CURB | (J) DIAGONAL HATCHING -
5" - 10" SPACING AT
ANGLE OF 30° TO 45° |
| (B) WORD LEGENDS (SEE
MUTCD - FIG. 9C-3) | (E) TACTILE WARNING
SURFACE | (H) 6" SKIP YELLOW | (K) REMOVABLE BOLLARDS |
| (C) STOP BAR - 24"
LEGEND WHITE | (F) CROSSWALK - 24"
LEGEND WHITE (SEE
DETAIL) | (I) DOUBLE CONTINUOUS
YELLOW STRIPE - 6" | (L) YIELD BAR - 20"
FROM CROSSWALK |

SHARED-USE PATHWAY

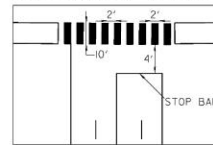


- | | |
|--|-------------------------------------|
| (A) RECOVERY AREA -
5' TYP/3' MIN | (C) PATH WIDTH -
14' MAX/10' MIN |
| (B) UNPAVED SHOULDER -
2", MAX SLOPE 4% | (D) 4" SKIP YELLOW STRIPE |
| (E) LATERAL CLEARANCE -
1' MIN | (F) 6" SKIP YELLOW STRIPE |

SIDEPATH CROSSING AT INTERSECTION

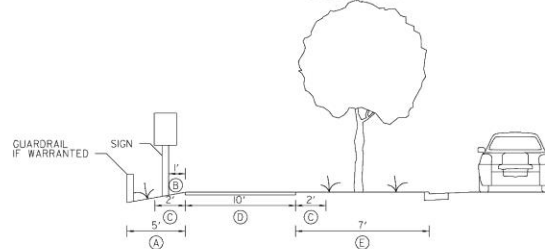


CROSSWALK DETAIL



- | | | |
|--|---|---------------------------------------|
| (A) BICYCLE SYMBOL
(SEE MUTCD -
FIG. 9C-3) | (D) CROSSING TO STREET -
8" MIN PREFERRED | (H) 4" SKIP YELLOW |
| (B) WORD LEGENDS (SEE
MUTCD - FIG. 9C-3) | (E) CROSSWALK - 24"
LEGEND WHITE (SEE
DETAIL) | (I) REMOVABLE BOLLARDS |
| (C) STOP BAR - 24"
LEGEND WHITE | (F) TACTILE WARNING
SURFACE | (J) YIELD BAR - 20"
FROM CROSSWALK |

SIDEPATH



- | | |
|--|--|
| (A) RECOVERY AREA -
5' TYP/3' MIN | (D) PATH WIDTH -
14' MAX/8' MIN |
| (B) LATERAL CLEARANCE -
1' MIN | (E) SEPARATION FROM
TRAVEL LANE - SEE
SD-1 FOR VERGE WIDTH |
| (C) UNPAVED SHOULDER -
2", MAX SLOPE 4% | (F) 6" SKIP YELLOW STRIPE |

DRAFT



SHARED-USE
PATHWAY
DETAILS

PIGEON FORGE
FILE STANDARD.dgn
DATE: 3/2/2018

SU-1
SHEET NUMBER
3

Diagram illustrating the calculation of the required width of a sidewalk (A+B+C+D) based on the width of the frontage (A) and the width of the pedestrian zone (B). The diagram shows a building frontage (A), a pedestrian zone (B) with a 1.50m width, a tree, a person walking, a person on a bicycle, and a car. The total width is the sum of A, B, C, and D.

LEGEND	
(A) FRONTAGE	(C) FURNISHING/VERGE
(B) PEDESTRIAN	(D) CLEAR

SIDEWALK ZONE CURBED			RURAL		SUBURBAN		URBAN	
			MIN.	TYP.	MIN.	TYP.	MIN.	TYP.
ARTERIAL	A	FRONTAGE	1	1	1	1	1	
	B	PEDESTRIAN	5	8	5	7	6	10
	C	FURNISHING	5	8	4	6	4	6
	D	CLEAR	5	8	1	2	1	2
COLLECTOR	A	FRONTAGE	1	1	1	1	1	4
	B	PEDESTRIAN	5	6	5	6	6	8
	C	FURNISHING	3	5	3	5	4	5
	D	CLEAR	2	4	1	2	1	2
LOCAL	A	FRONTAGE	0	1	0	1	0	3
	B	PEDESTRIAN	5	6	5	6	6	8
	C	FURNISHING	3	4	3	5	4	5
	D	CLEAR	1	3	1	1	1	2

Diagram illustrating the 1.50% rule for shoulder width. The diagram shows a cross-section of a road with a tree on the left, a sidewalk with two pedestrians, a bicycle lane with a cyclist, and a car lane with a car. A dimension line labeled "1.50%" indicates the width of the shoulder area. Below the diagram is a legend defining the components: (A) SHOULDER, (B) SIDEWALK, (C) VERGE, and (D) CLEAR.

SIDEWALK ZONE UNCURBED			RURAL		SUBURBAN	
			MIN.	TYP.	MIN.	TYP.
ARTERIAL	A	SHOULDER	1	2	1	
	B	SIDEWALK	5	8	5	8
	C	VERGE	6	10	5	8
	D	CLEAR	7	10	5	8
COLLECTOR	A	SHOULDER	1	2	1	2
	B	SIDEWALK	5	7	5	7
	C	VERGE	5	8	4	6
	D	CLEAR	4	7	4	6
LOCAL	A	SHOULDER	0	1	0	1
	B	SIDEWALK	5	6	5	6
	C	VERGE	3	5	3	5
	D	CLEAR	2	4	2	4

Diagram illustrating the cross-section of a curb and gutter assembly, showing the flow of water from the street into the gutter and then into the planter area.

Labels and Dimensions:

- 6" HIGH CONC. CURB**: The vertical curb separating the street from the planter area.
- 4" OPENING IN CURB TO ALLOW DRAINAGE INTO PLANTER**: The opening at the base of the curb.
- CHECK DAMS AS NEEDED LOCATIONS VARY**: Indicated by arrows pointing to the curb structure.
- STREET**: The area on the left side of the curb.
- LENGTH VARIES**: Dimension line indicating the length of the curb assembly.
- FLOW**: Arrows indicating the direction of water flow from the street into the gutter and then into the planter area.
- 1X TYP.**: Typical slope dimension.
- VARIES MIN. FOR TREE PLANTING**: Dimension line indicating the width of the planter area.
- CONCRETE SPLASH PAD OR RIVER COBBLES**: Material used in the planter area.

Diagram illustrating the plan view of a park layout. Key features and dimensions include:

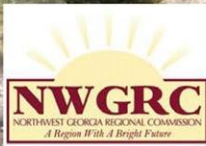
- Path:** A curved path with a 15' RADIUS TYP. and a 15' TYP. segment.
- BENCH:** Located near the path.
- TREE PLANTING PIT:** Indicated by a circle with a cross.
- PEDESTRIAN LIGHT:** Indicated by a circle with a cross.
- BKE RACKS:** Indicated by a circle with a cross.
- Dimensions:**
 - 15' TYP. (Path segment)
 - 7' (Bench width)
 - 15' (Bench length)
 - 30' TYP. (Tree planting pit spacing)
 - 8' (Pedestrian light spacing)
 - 60' TYP. (Bike racks spacing)
 - 15' (Bike racks length)
- Labels:** BENCH, TREE PLANTING PIT, PEDESTRIAN LIGHT, BKE RACKS.

	SIDEWALK DETAILS	PIGEON FORGE	SD-1 SHEET NUMBER 4
		FILE: STANDARD.dgn	
		DATE: 3/2/2018	

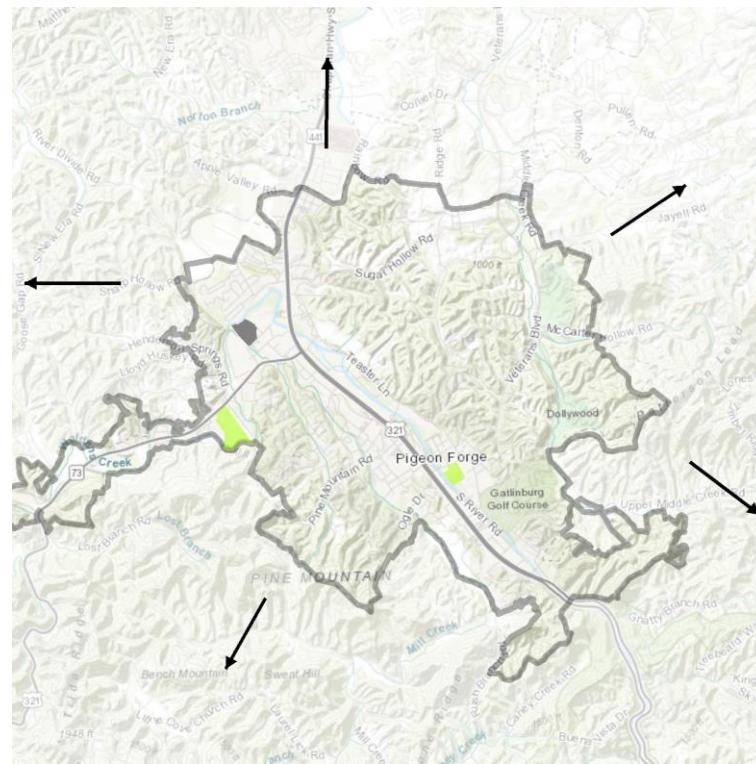




- Generates \$120 million annually
- Supports 1,300 jobs along 66-mile route

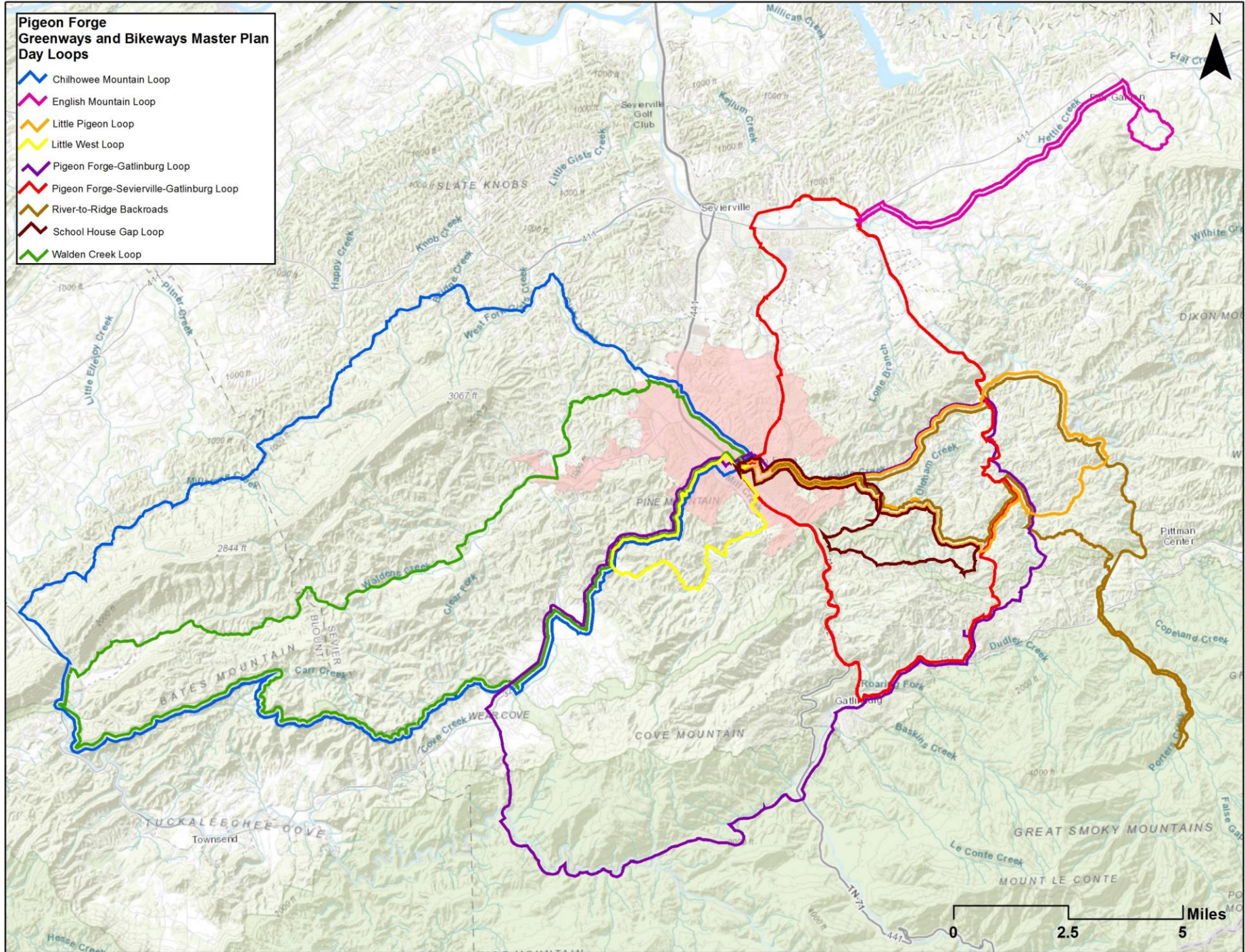


Silver Comet Trail
Economic Impact Analysis and
Planning Study
July 2013



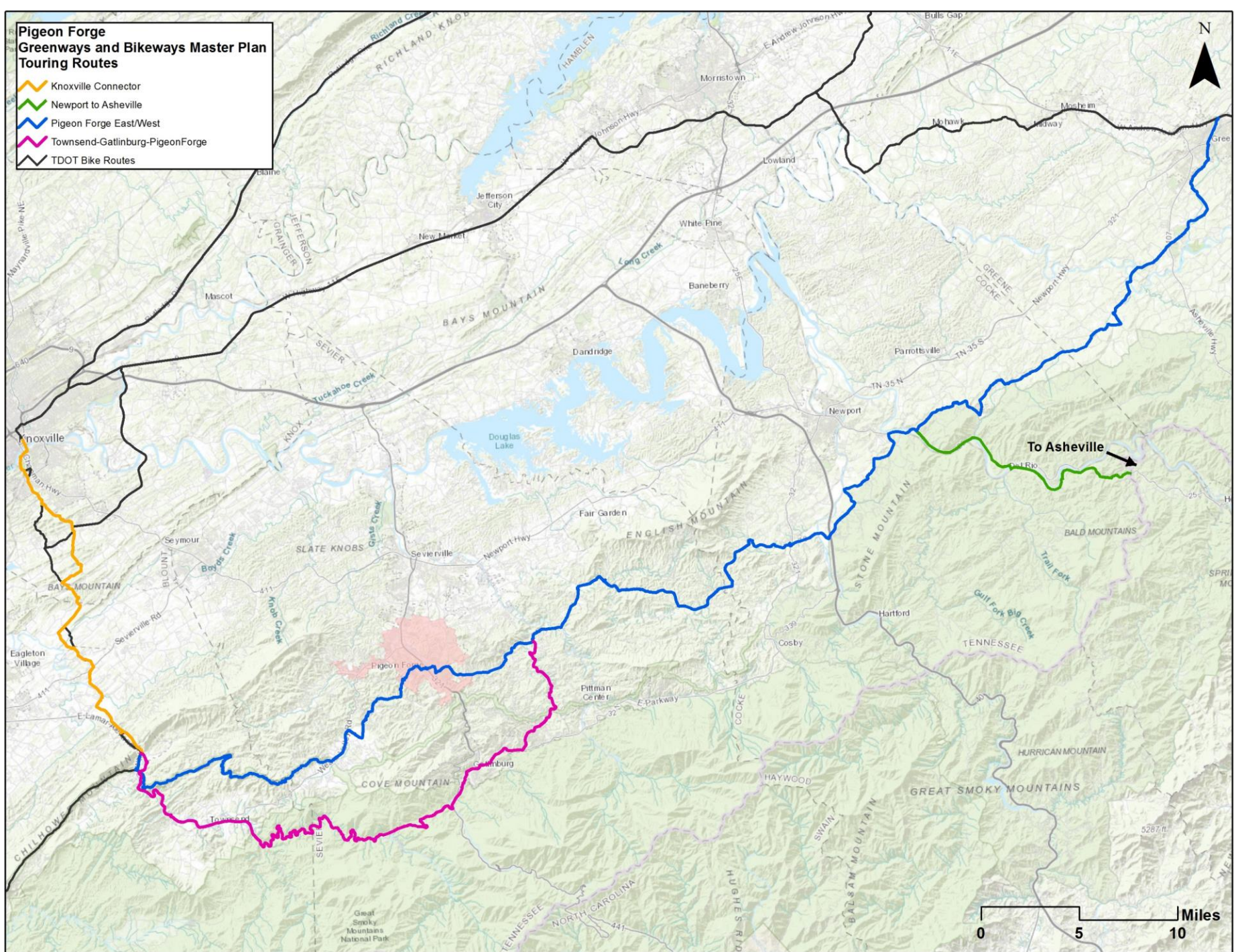
Pigeon Forge Greenways and Bikeways Master Plan Day Loops

- Chilhowee Mountain Loop
- English Mountain Loop
- Little Pigeon Loop
- Little West Loop
- Pigeon Forge-Gatlinburg Loop
- Pigeon Forge-Sevierville-Gatlinburg Loop
- River-to-Ridge Backroads
- School House Gap Loop
- Walden Creek Loop



Pigeon Forge Greenways and Bikeways Master Plan Touring Routes

- Knoxville Connector
- Newport to Asheville
- Pigeon Forge East/West
- Townsend-Gatlinburg-Pigeon Forge
- TDOT Bike Routes







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OUR PROJECTS

ZYP BIKESHARE (BIRMINGHAM, AL)

West persistently worked to launch bike share in Birmingham by building community support, political will, and private sector sponsorship. West, as the Director of Zyp, executed all facets of actually operating a year-round bike share system.

BATON ROUGE BIKESHARE (BATON ROUGE, LA)

Bentam worked with Toole Design Group and Baton Rouge Area Foundation on the finalization of the 2016 Feasibility and Implementation Plan for the system. Bentam is contracted to manage and implement the bikeshare plan, which includes government affairs, community engagement, and sponsorship procurement. Bentam completed the federal and local funding applications and is currently assisting the City-Parish with Request for Proposals for the capital equipment, installation, and site planning for the system. The system is expected to launch in Spring 2019.

ST. TAMMANY PARISH (ST. TAMMANY PARISH, LA)

Bentam is currently consulting for St. Tammany Parish which focuses on bringing bike share to five communities along the 30-mile Tammany Trace and will serve the tourist and residential population with active transportation.

BAYOU BIKESHARE (LAKE CHARLES, LA & SULPHUR, LA)

Bentam is currently consulting with the Cities of Lake Charles and Sulphur, LA including

Multi-city bikeshare program coming to East Alabama

by Byron Khalil | Tuesday, October 23rd 2018

AA



(abc3340.com)



CALHOUN COUNTY, Ala. — Three cities in Calhoun County are partnering for something that's never been done before in Alabama. It's the first multi-city bikeshare [program](#).

"It's so exciting because you get to explore and be outside, and you're exercising, which I love to do," Emily Jones, a local Calhoun County cyclist said.

She calls Oxford, Anniston, and Jacksonville joining forces to create a bikesharing program a winning idea.



PIGEON FORGE GREENWAY/BIKEWAY MASTER PLAN



PIGEON FORGE GREENWAY/BIKEWAY MASTER PLAN

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**TAPA Spring Retreat
March 22, 2019**